

February 12, 1962

TO: ALL ROOTES GROUP DEALERS

WINDSHIELD ASSEMBLY - FITTING AND REMOVAL

SUNBEAM ALPINE

This bulletin supersedes Data Sheet No. 14 of WSM.124 and Service Bulletin 205 of this volume.

Many Alpines are shipped overseas with the windshield assembly removed and the following instructions must be complied with to ensure a watertight windshield and a good soft top or hardtop fit.

This information will also be of assistance when windshields are refitted in Service.

To fit

Before fitting the windshield assembly to the car, remove the expendable tonneau cover studs from the crash pad, and the tape from the windshield assembly mounting holes and demister slots. It is important that only the assembly shipped with a car is fitted to that car; under no circumstances should windshield assemblies be interchanged.

Place the windshield assembly temporarily in position on its mounts after fitting the windshield base weatherstrip and the door window supports. The windshield base weatherstrip, door window supports, nuts and washers for the retention of the windshield assembly will be found in a package in the trunk of the car. Before fitting the retaining nuts and washers, apply a recommended sealing compound to the top face of the large plain washers (Fig. 1.).

Check that the dimension between the stud on the inside top corner of the windshield pillar and the hole in the pivot bracket on the door shut pillar is to the specified measurement of $29\frac{1}{2}$ ins. as shown in Fig. 2.

If this dimension is more, or less, than the specified amount, then the windshield assembly must be packed with shims (Part No. 2201893) at the four end studs to achieve the required degree of tilt. When the dimension is correct, a liberal amount of sealing compound is applied between the windshield base weatherstrip and the scuttle top (Fig. 3 Op. 1).

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After the required measurement has been achieved, tighten up the windshield assembly retaining nuts (two at each side and three across the front), taking care not to overtighten them as this may split the rubber weatherstrip at the scuttle top. Recheck the measurement after tightening the nuts.

Raise the soft top and engage the two toggle clamp spigots on the front rail to the toggle clamp spigot sockets on the windshield top channel, but do not yet fasten the clamps. Ensure that the front rail weatherstrip contacts continuously at the windshield top channel to form an adequate seal against water entry.

If the weatherstrip is wrinkled or folded under itself, a new weatherstrip (Part No. 2207539) must be fitted. The seal must be located centrally and not stretched during fitting.

When correctly fitted, the ends of the seal should cover and compress on to the windshield side pillar seals and should protrude beyond them by approximately $\frac{1}{2}$ in. as shown in Fig. 3 (a).

In the unlikely event of more nip being required at the header rail between the windshield frame and the header rail weatherstrip, further packing (Part No. 2201875) may be fitted under the toggle clamps in such a position as to give a parallel instead of a wedge-shaped packing. (Put the tapered ends of the wedges towards each other).

After successfully fitting the soft top, the sealing between the windshield side pillars and the door window supports should be checked in the following manner.

In order to ensure a satisfactory seal between the door window support and the windshield side pillar, there should be $\frac{1}{4}$ in. gap from the forward edge of the door windshield side pillar. This measurement should be consistent throughout as shown in Fig. 4 (a). If the side pillar weatherstrip (Fig. 4 b) has acquired a permanent "set" due to misalignment or crumpling it should be renewed. Further, the side pillar weatherstrip should be trimmed flush at its lower edge so that it just brushes on the top of the door window support when the door is closed. This latter item can be applied to both Series I & II cars.

Finally, test for water leaks and fit the wiper arms in their correct positions as shown in Data Sheet No. 12 (WSM.124) or Service Bulletin 62/1, whichever is applicable.

The following information will be useful whenever a windshield glass has to be renewed.

To remove

It is recommended that if the windshield has to be removed for any reason, the complete assembly is taken off the car and windshield

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removed from its frame on a bench.

If a hardtop is fitted this must be removed.

Remove the windshield wiper arms.

From below the crash pad, take off the nuts and washers which retain the windshield wiper assembly in position.

With the assembly removed from the car, take off the top and bottom channels and the side pillar by removing the eight retaining screws. Alternatively, remove the top channel only and spring the side pillars apart.

If the latter method is used, care must be taken to ensure that the screws which retain the bottom channel to the side pillars are not damaged.

Remove the weatherstrip from the glass.

To re-assemble

Remove all traces of old sealing compound from the glass and weatherstrip.

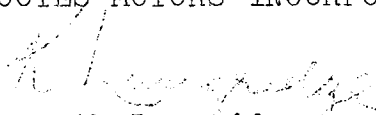
Reference should be made to Fig. 5 as to the correct way to fit the weatherstrip to the glass and bottom channel. "A" shows early cars and "B" later cars. It should also be noted at this juncture that the wide notches in the rubber are at the top.

Apply a coat of sealing compound to the area of the weatherstrip which is in contact with the outside face of the glass (Fig. 3 Op. 2), and fit weatherstrip to the glass.

Replace top and bottom channels and the side pillars (depending on which method was used to remove the glass) and refit the retaining screws.

NOTE: All of the foregoing instructions apply equally to both Series I and II cars except in the case of the windshield side pillar to door window support. The window supports are different on Series I and II cars.

ROOTES MOTORS INCORPORATED


Kenneth Lengridge
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Encl: Sketches

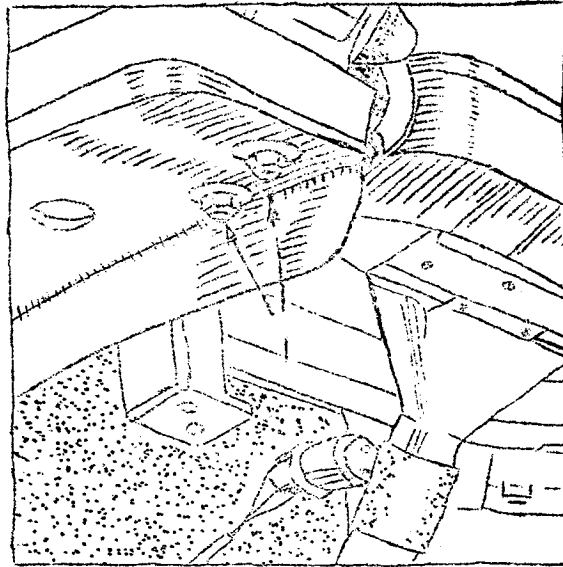


Fig. 1. Windscreen retaining nuts

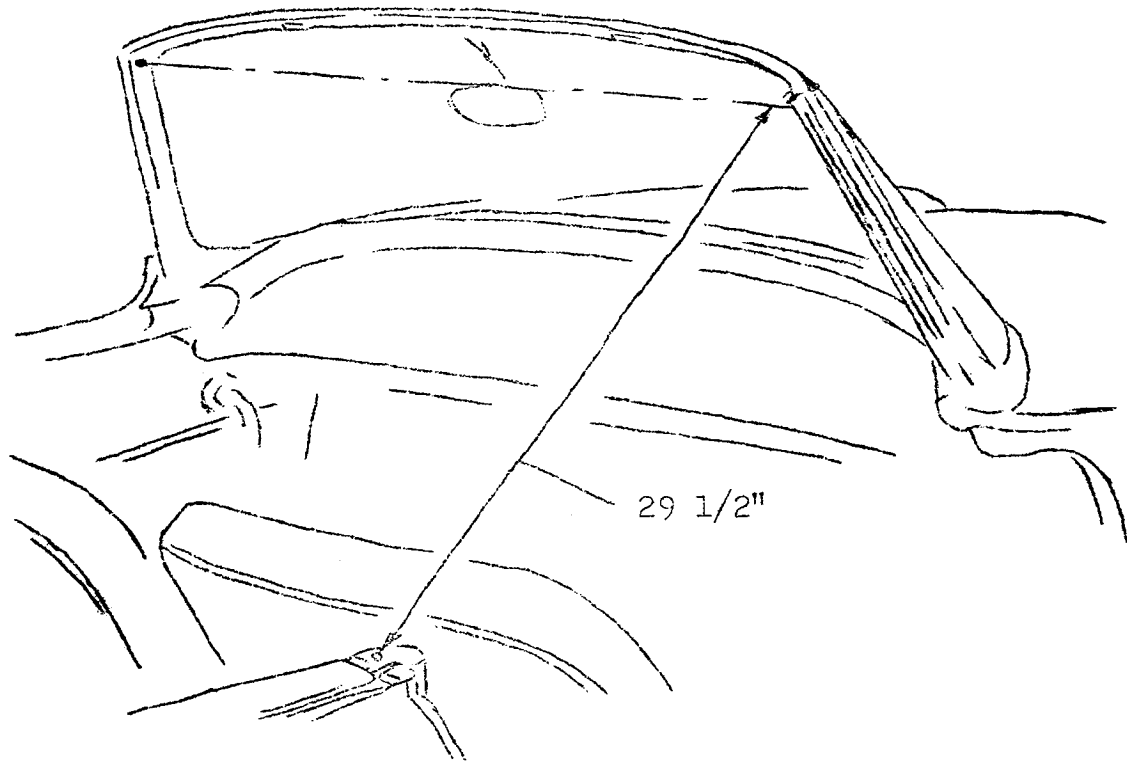


Fig. 2. Showing correct measurements between screen pillar and pivot bracket.

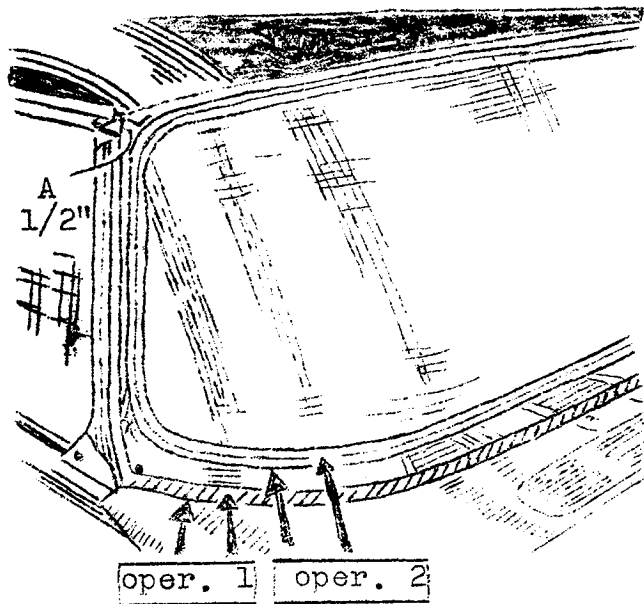


Fig. #3. Windscreen sealing

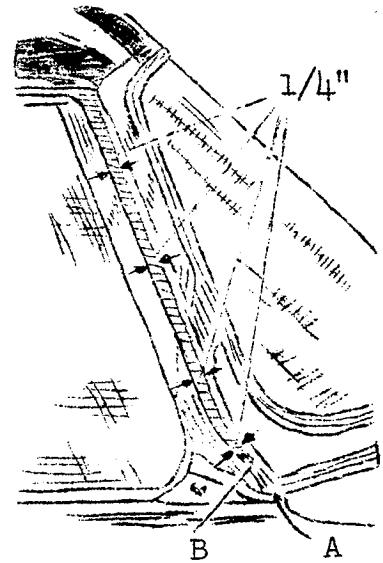


Fig. 4. Correct relation of windscreen side pillar weatherstrip to chrome door support.

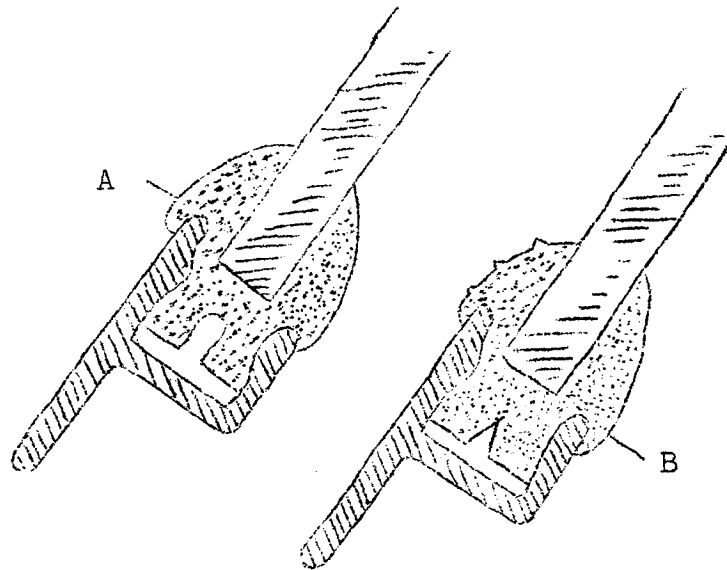


Fig. 5. Correct way of fitting weatherstrip to the windscreen and bottom channel.